

M and B WASTE MANAGEMENT & TRANSPORT LOGISTICS

Whilst studying for my PhD I supervised a final year student James Hoyland whose father owned M and B WASTE MANAGEMENT & TRANSPORT LOGISTICS. James final year project was to improve the fuel consumption of the vehicle fleet. As his project progressed it became apparent that two recently purchased new rigid vehicles had very poor fuel consumption. Having checked the accuracy of the in-cab fuel consumption data against the fuel refill data from the on-site bulk fuel tank it was confirmed that the fuel consumption was indeed poor and the worst in the fleet.

Examination of the spreadsheet containing the fuel records showed that the vehicles were only travelling short distances every day. The company had a policy of all vehicles being refuelled every evening on return to depot. This is an important part of data management. The more data you have the greater the insight into the performance of the vehicles. The next step was to review the specification of the two vehicles. The vehicles had been bought at a discounted price due to them being part of a cancelled order. These two vehicles had been specified with 380 horsepower engines.

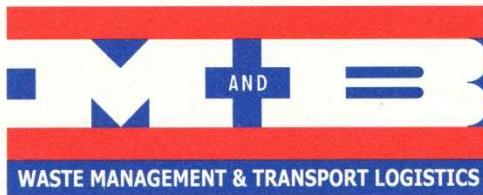
Based upon the work profile of these two vehicles, which essentially was running around locally; urban roads, short distances and multiple drop off and pick-up it was evident that the engines were over specified. The engines were controlled through an electronic management unit, sometimes referred to as an engine map or module. The vehicle distributor was asked to change the engine maps on the vehicles to 310 horsepower.

The fuel consumption improved immediately. The first vehicle recorded an improvement in fuel consumption of 40% (see letter on the next page) and the second vehicle recorded an improvement of 17%. These improvements were sustained.

This shows the importance of several aspects of fleet and fuel management including:

1. Having accurate fuel consumption data.
2. Having daily or shift fuel consumption.
3. Correct vehicle specification.

Normally, when engine re-mapping, or tuning or chipping is discussed it is with a view to increasing torque and power. In this case, attention to detail and the combination of vehicle engineering knowledge and the knowledge of the local management resulted in a significant improvement in fuel consumption. Improvements in fuel consumption of this magnitude result in a major improvement in vehicle and company financial performance.



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RE: Letter of recommendation

Dear Michael

Whilst I was undertaking my final year project at Huddersfield University, your assistance with M&B's fleet fuel consumption was invaluable in improving several of our vehicles performance, one of which improved by as much as 40%.

I would have no hesitation in recommending your services to any LGV fleet operator.

Yours sincerely

A handwritten signature in black ink, appearing to read 'James Hoyland'.

James Hoyland

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